



"The engine is the heart of the tractor, if it's the next-generation it's even better."

"A tractor with unexpected performance. A great little tractor."



DORADO

80 | 90 | 90.4 | 100.4
Classic 80 | 90 | 90.4



SAME

SAME DORADO. A GREAT LITTLE TRACTOR.

A family of versatile and compact tractors, born to face working operation in the open field, company applications and jobs where compactness is absolutely essential.

The Dorado range is very broad and encompasses Tier4i models equipped with modern FARMotion engines.

The SAME Dorado family offers an impressive variety of configurations, superior comfort and modern features to take on any job with ease: from the smallest in the range, which is perfect for general farm tasks - even in confined spaces - to the most powerful, designed for heavier duty jobs which need more PTO power.

The choice of 2 and 4-wheel drive versions, 3 or 4 cylinders and standard or Classic models make the range ideal for use in small and medium-sized farms, even in hilly and mountainous areas, where the Dorado is able to perform any job in safety, thanks to its low centre of gravity stability and 4-wheel braking.







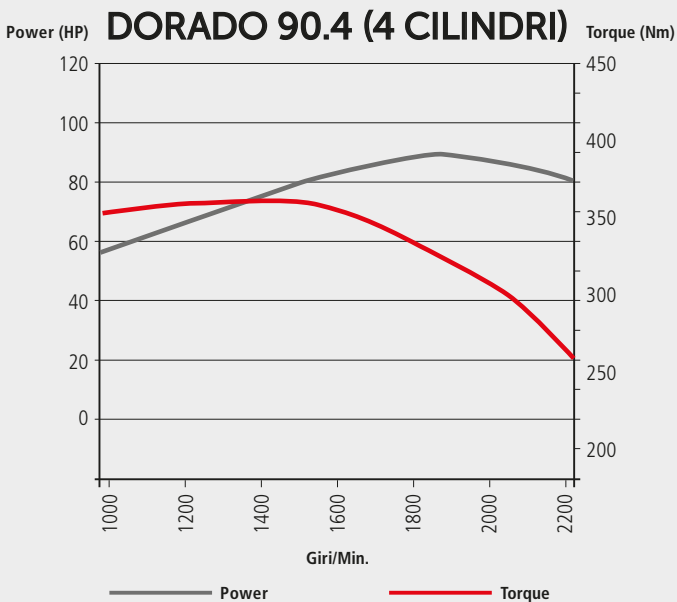
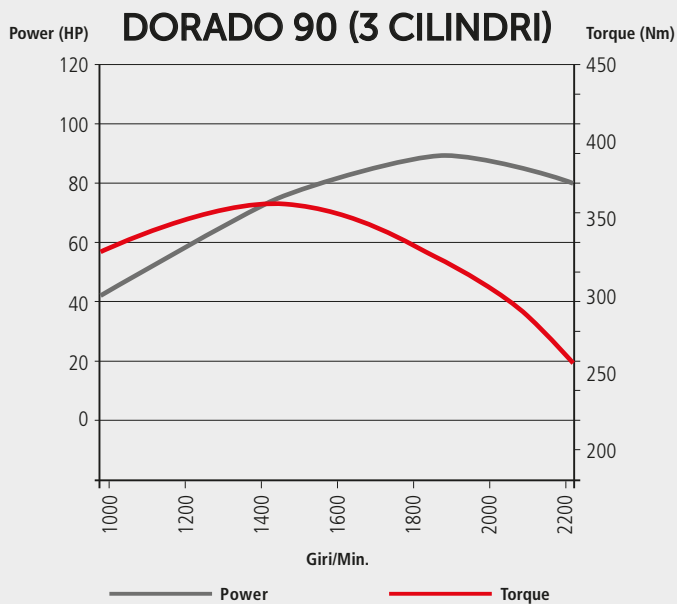
"You notice immediately when you have an engine specifically designed for agriculture."

BROAD SELECTION OF 3 AND 4 CYLINDER ENGINES

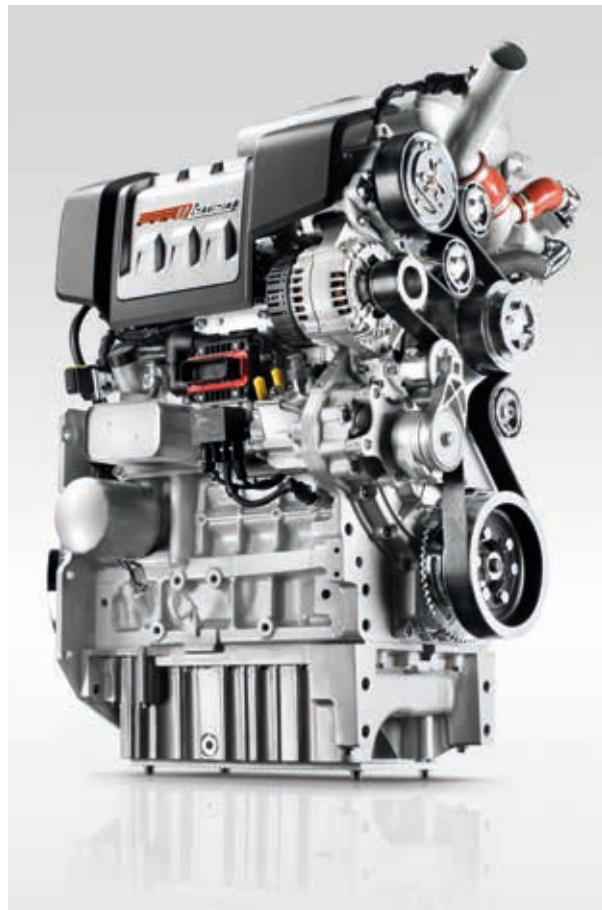
The heart of the Dorado Tier4i model is represented by the advanced FARMotion engines, developed and produced by SDF.

Compliance with the current regulations on Tier4i emissions levels is guaranteed by the EGR, the exhaust gas recirculation, combined with the DOC catalytic converter. Even as far as its design, innovative technical features were applied: the capacity has been reduced, the Common Rail injection system guarantees instantaneous injection and a particularly high (2000 bar) operating pressure. All engines have hydraulic tappets for injection timing management. The intercooler cools the air of the turbo. The increase in the amount of air improves combustion and makes it more efficient, with a number of advantages: increased power, reduced emissions and lower engine operating temperature. The manufacturing of the crankshaft with a new design and is made of forged steel for greater durability, at low engine speeds this ensures less wear and tear of moving parts, whilst at the same time reducing noise for greater driver comfort. The wiring and all the various components under the hood have been located for

greater protection and durability, as well as to allow access to the various service points. All facilitated by a monobloc hood. Also the redesigned air flow to the new cylinder head and combustion chamber geometry, contributes to improved fuel combustion, letting Dorado tractors take full advantage of the engine performance while significantly reducing fuel consumption.



**COMMON RAIL
2,000 BAR**



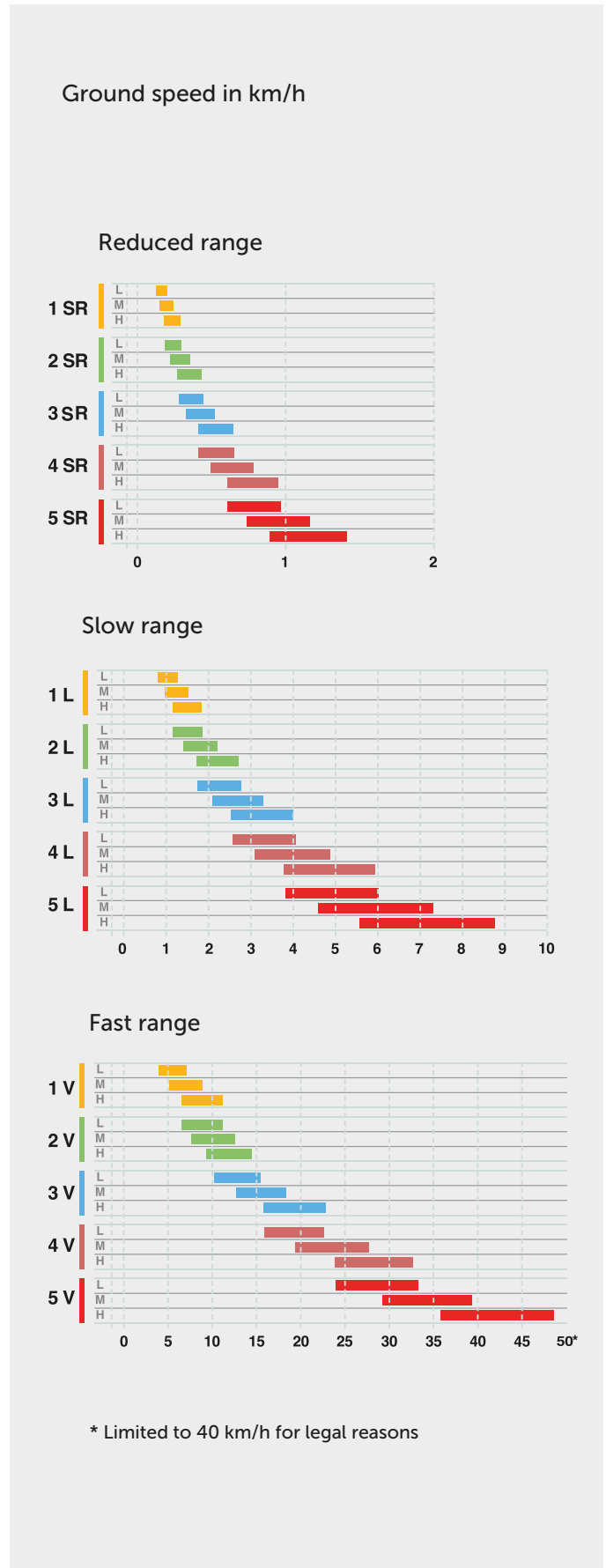
ELECTRONIC MANAGEMENT TOTAL CONTROL

All models have electronic engine management: an advanced feature in which the fuel supply is always correct, depending on engine speed and load, thus optimising consumption. With the aid of specific sensors, the electronic control unit monitors all operating parameters, constantly setting them to the optimal values via injection management. Optimum fuel delivery is ensured to provide consistent performance. Thanks to this design, the rated engine speed is 2200 rpm, with constant torque. All this allows optimum power availability with very low consumption. This function also allows a minimum and maximum engine speed to be set, stored and retrieved, simplifying headland turning operations.

MAXIMUM EFFICIENCY WITH POWERSHIFT AND OVERSPEED.

POWERSHIFT

Versatility and efficiency are the two main characteristics of the SAME Dorado, thanks to the transmission that can offer up to 3 ranges (with super-creeper) and 5 gears with 3 Powershift speeds, for a total of 45 forward and 45 reverse speeds. For a more traditional specification, you can opt for an easy-to-use and just as functional mechanical gearbox. With the Powershift gearbox, the driver has the possibility to change speed quickly, without pressing the clutch pedal, optimising ground speed in conjunction to the job at hand: in this way, the transmission loading can be reduced, maintaining optimum engine speed. Moreover, by pressing the "Comfort Clutch" button on the shift lever, gear shifts are performed smoothly and progressively without pressing the clutch pedal, offering even greater comfort.



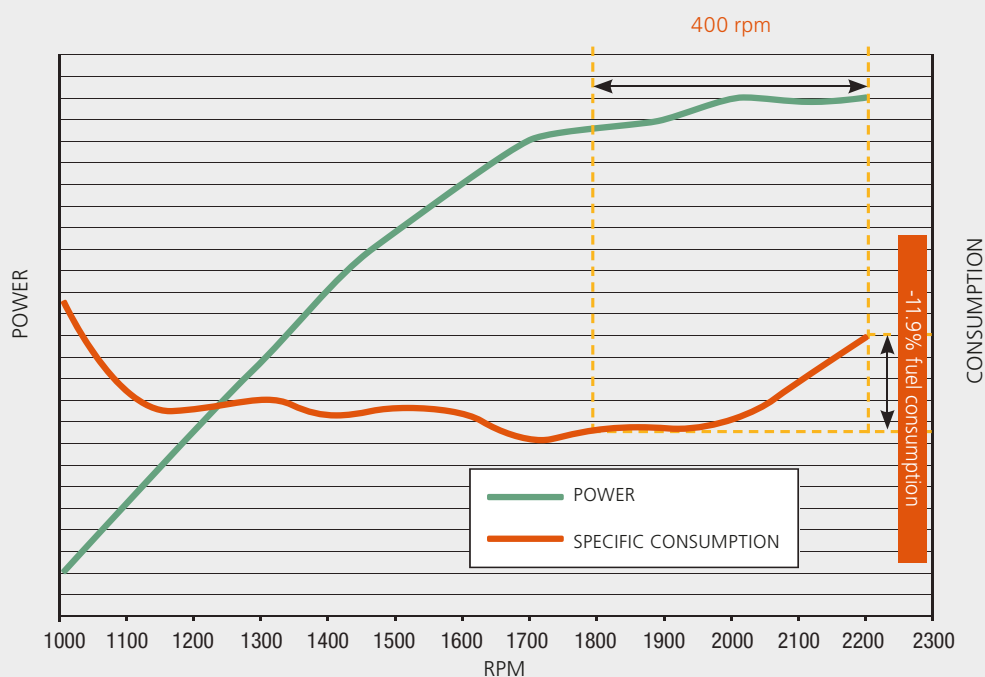
OVERSPEED

But that's not all. For maximum performance, Overspeed is available: a gearbox with well spaced transmission ratios in order to potentially reach 50 km/h, electronically limited to 40 km/h. The Overspeed allows road travel at 40 km/h, both with economical engine rpm (in top gear) as well as fully exploiting engine performance, using the lower gear. The low engine speed reduces fuel consumption while at the same time allowing the engine to be used at a speed close to that of maximum torque. All this translates into an optimal power curve, reduction of fuel consumption and enhanced driving comfort, thanks to lower noise and less vibrations. Finally, the Overspeed allows the maximum speed of 40 km/h to be reached, regardless of the size of the tyres used, significantly increasing the efficiency and comfort of tractor in road transport and travel.



On average, the "Overspeed" gearbox, in 5th gear with "fast" range at 40 km/h, has an engine speed 400 rpm lower than the maximum power speed with fuel savings of around 11/12%.

OVERSPEED



POWER SHUTTLE WITH STOP&GO. GOODBYE CLUTCH!

With the Power shuttle, SAME provides an industry leading innovation: The Stop&Go system. This feature extends the characteristics and potential of the shuttle, providing the driver with a higher level of manoeuvrability, especially when it is necessary to stop the tractor at irregular time intervals, such as, for example, in operations with the front loader, when hooking up implements or during hill starts. With the Stop&Go function, the driver can in fact stop the tractor and set off again by simply using the brakes, without using the clutch pedal. Technically the system is managed by an electronic control unit that processes the information received from the shuttle lever on the steering column, from the braking system and from a sensor situated in the rear part of the gearbox,

recording the ground speed of the tractor. Based on the signals received, the control unit is able to control (via special solenoid valves) the starting or temporary stopping of the shuttle, without the need to use the clutch pedal. In practice, acting on the brake pedal, the tractor stops and, at the same time, the system puts the shuttle "on hold". Then lifting the foot from the brake pedal, the Stop&Go re-activates the shuttle manoeuvre, ensuring gradual and modulated engagement of the hydraulic clutch (one for the forward and one for the reverse speed) and thus the drive of the tractor positively without any jerks. All this gives the tractor great handling when manoeuvring, in addition to greater safety, reliability and driving comfort.





Power shuttle section with Stop&Go system



POWER SHUTTLE

To complement the higher specification versions, the Power shuttle is available: with 2 "Long Life" multiplate wet clutches (for greater durability and reliability) electronically controlled which enables direction change under load, below 13 km/h, while ensuring minimum loading of the mechanical parts involved. This feature allows quick directional change when undertaking manoeuvres and headland turns. Situated under the steering wheel, the ergonomic and easy to use shuttle control lever also has a neutral position and an interlock function for working in total safety.



"I'm looking for maximum efficiency, also in terms of cost."

**POWER TAKE-OFF.
GREAT VERSATILITY**

The versatility of the Dorado range also extends to the choice of PTO speeds offered: with multiplate wet clutch, 540/540 ECO/1000 rpm speed and ground speed PTO. This means that the tractor can work with a wide and varied selection of implements with unrivalled economy and productivity. The Dorado is the ideal choice for PTO applications, also profiting from the low weight/power ratio. The PTO selection and engagement is extremely simple: the controls are ergonomic and easy to use, whilst a dedicated active digital display on the instrument panel shows the operating speed.

**STRAIGHTFORWARD AND EFFICIENT
MANAGEMENT OF ALL IMPLEMENTS**

The Dorado size and versatility will mean these tractors will be asked to handle a wide variety of implements. For this reason the hydraulic system has been designed to ensure simple logical operation of all tasks with ease and precision.

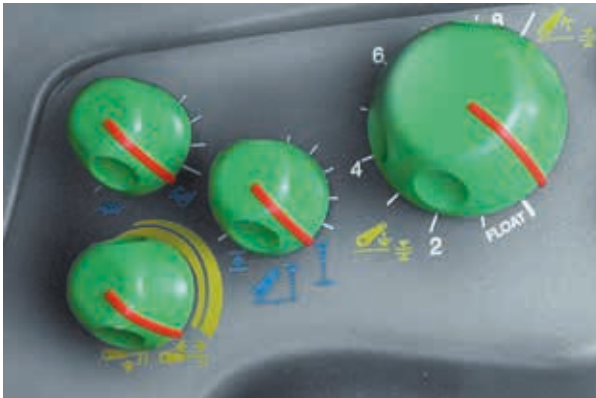
The hydraulic system includes a 54 L/min pump for the robust rear hitch and three auxiliary valves, allowing full operation of all hydraulically controlled and adjusted implements. The auxiliary valves have a flow regulator, providing further possibilities of use and optimum flow output. The hydrostatic steering, on the other hand, has an independent pump ensuring smooth handling, even at low engine speeds, a situation typical when doing general farm jobs around the yard.



ELECTRONIC HITCH POWER AND PRECISION

For high precision jobs, the electronic rear hitch can control implements with extreme accuracy and precision. Along with this, the auto PTO, which automatically engages and disengages when the implement is lowered or raised, is also available. With a particularly robust design, the rear hitch of the Dorado has a lifting capacity 3000 kg, which can be increased to 3600 kg when fitted with optional assistor rams. Rear fender hitch controls can also assist when coupling an implement to the rear linkage. For those requiring

front-mounted implements, these tractors may also be equipped with a front hitch (with a load capacity of 1750 Kg) and a 1000 rpm front PTO: an attachment which will further extend the capabilities of the Dorado. It can also be used for carrying front ballast for the tractor.



"Comfort and complete visibility, that's what I want."

TOP CLASS COMFORT

The concept of comfort, in every power category, is always top priority for SAME.

The Dorado is designed to make the working day comfortable and pleasant for the driver, who can then operate the full potential of the tractor. The four-post cab is one of the strengths of Dorado: generous space inside and equally generous visibility in all directions. Less stress, with precise and safe control of the job in hand. Familiarising yourself with a Dorado is very easy.

All the controls are arranged in a logical and functional manner, grouped to the right of the driver in a comfortable working position.

In the high specification versions, engagement of the PTO, 4WD and differential lock is by means of electrohydraulic touch-button controls. The Powershift speeds and comfort clutch control are operated by buttons situated on the gear lever. The instrument panel provides a wide range of indicators and warning lights to ensure total control of tractor operation, with illuminated displays which are clearly visible in any light conditions. Data on the ground speed, the PTO speed, the hours of work and the distance travelled are provided in real-time to enable a rapid analysis of the work carried out.



A MADE-TO-MEASURE DRIVING SEAT

The Dorado offers a driving seat which can perfectly adapt to the driver's characteristics, ensuring automotive-level comfort. The air suspension seat with seat belt adapts perfectly to the weight and height of the driver and provides a good range of positioning movement. The comfort of the flat platform suspended on silent blocks, the suspended hydraulically controlled clutch and brake pedals and the lateral gear shift levers further alleviate fatigue, as too does the layout of the controls in the console to the right of the driver. The special curvature of the



windows, together with the narrow and tapered hood, ensure greater driving comfort and optimum visibility in all directions.



“For me, simplicity and reliability are what I require.”

“With all the controls at my fingertips, every job seems easier.”



THE RIGHT CLIMATE FOR EFFECTIVE WORKING

The Dorado offers an additional level of comfort, thanks to the reduced in-cab noise levels and pressurised cab, also with air conditioning. With the high-visibility roof, the air conditioning system is situated in the rear part under the roof, whilst the air distribution vents are also positioned towards the floor, with 4 adjustable vents, in addition to the 4 vents in the upper part of the roof. Over the entire glass area, demisting and defrosting is particularly effective. For applications with front loaders, the high-visibility roof is also available which, in addition to ensuring complete visibility of the full lifting arc of the loader, ensures improved distribution of air in the cab.

DORADO COMFORT MEANS, ABOVE ALL, UTMOST SAFETY

Comfort for Dorado also means utmost safety. The Dorado incorporates an integral hydrostatic braking system on all four wheels, implemented with wet disc brakes, also on two-wheel drive versions. The system ensures safe and effective stopping of the tractor at high speed and with heavy loads. The soft and light hydrostatic control requires only minimal pedal pressure.

Finally, for total safety even when at rest on sloping ground, the Dorado has a completely independent parking brake, using wet discs to lock the transmission. The SAME Dorado is a revolutionary tractor concept, versatile and fully adaptable to the needs of those who use it. The ideal answer for those seeking a reliable and versatile partner.





**DORADO CLASSIC.
SURPRISING VERSATILITY**

The Dorado range has been further extended in the medium-low power segment with a simple, reliable and economical tractor such as the Dorado Classic. This tractor plays a leading role, not only for small farms as the main working tractor, but also in medium-sized ones to complement the larger models. These are "all round" machines which, due to their versatility, the farm's tractor drivers often find themselves using.

**SIMPLE HIGH PERFORMANCE
TRANSMISSIONS**

The high efficiency of the mechanical transmissions is combined with a good spacing of speeds, thanks to the 5-speed gear shift that provides a suitable selection of gears in each range.

The Dorado Classic has a synchronised mechanical gearbox with 5 speeds in 2 ranges, for a total of 10 forward and 10 reverse speeds and synchronised reverse shuttle. The option of a synchronised mechanical gearbox with 5 speeds in 3 ranges - with supercreeper gear and underdrive - provides 30 forward and 15 reverse speeds. To make road transport and travel quick and easy, for both transmissions the maximum speed is 40 km/h in the 4WD and 2WD versions with front brakes. The availability of 3 PTO speeds (540 - 540ECO - 1000) allows full power to be delivered with maximum reliability. For more demanding types of transport, the ground speed PTO can be used which, for the Dorado Classic, has an independent output shaft. Engagement of the rear PTO is mechanical by means of a lever on the left side of the seat.

HYDRAULIC HITCH: SIMPLE AND PRECISE

The hydraulic hitch controls rear implements effectively, with precise position, draft and mix control. The hydraulic flow (54 L/min) is good for the category of machinery and the 2 or 3 valves manage the control and adjustment of the hydraulic implements. For greater implement coupling convenience, a 3-point hitch is also available with automatic quick coupling linkage. Lifting capacity of 3600 kg with assistor rams.

UTMOST SAFETY IN ALL CONDITIONS

On hills and on the most difficult terrain, the SAME Dorado Classic is designed to operate in a safe and secure way. The sufficient ground clearance, correct weight distribution and effective front-wheel drive (engaged by a lever on the left side of the seat), with wet disc brakes, ensure productivity and short stopping distances, even with heavy mounted implements. In addition to 4-wheel braking, the differential lock (100% front and rear) ensures maximum traction even in the most adverse conditions. The hydrostatic steering has a completely independent pump, capable of ensuring light steering even at low engine speeds.



CAB: A PROTECTED AND SAFE ENVIRONMENT

The Dorado Classic range has a flat platform suspended on Silent-Blocks and cab. The driving area is spacious, comfortable and the driver can easily get on and off. The correct driving position is ensured by suspended pedals and adjustable steering wheel.

On the SAME Dorado Classic the cab provides a working environment with low noise levels - noise levels are kept low with the use of the latest soundproof materials – and from dust, thanks to a large air filter and good sealing doors and windows. Accessibility and interior comfort are ensured by doors with a wide opening angle, strengthened glass windows, driving seat with armrests and, optional air suspension. Good all-round visibility is ensured by large windows and narrow cab posts. Heating, air conditioning, digital clock, radio ready, sun blind, front and rear work lights and rear windscreen wiper complete the standard specification.



Dorado

		80	90	90.4	100.4
ENGINE					
Emissions		Tier4i	Tier4i	Tier4i	Tier4i
Model		FARMotion	FARMotion	FARMotion	FARMotion
Cylinders/Displacement/Valves	No./cc/No.	3 / 2887 / 2	3 / 2887 / 2	4 / 3849 / 2	4 / 3849 / 2
Common Rail	bar	2000	2000	2000	2000
Turbo intercooler		●	●	●	●
Maximum power (ECE R 120) @ 2,000 rpm	kW/hp	55,4/75	65/88	65/88	75/102
Power at rated speed (ECE R 120) @2,200 rpm	kW/hp	55,4/75	61,6/84	61,6/84	71,1/97
Max. torque @ 1,600 rpm	Nm	341	354	354	408
Torque rise	%	40	32	32	32
Electronic engine control		●	●	●	●
Exhaust gas after-treatment system		DOC / exEGR	DOC / exEGR	DOC / exEGR	DOC / exEGR
Fuel tank capacity	litres	100	100	100	100
SHUTTLE					
Mechanical Shuttle (LS)		●	●	●	-
Hydraulic reverse power shuttle (GS)		●	●	●	●
Stop&Go System (GS)		●	●	●	●
MECHANICAL GEARBOX					
Number of speeds	n°	-	-	-	-
Number of speeds with supercreeper gear and underdrive	n°	30 + 15	30 + 15	30 + 15	30 + 15
Overspeed (40km/h Eco)		●	●	●	●
POWERSHIFT GEARBOX					
Number of speeds with supercreeper gear	n°	45 + 45	45 + 45	45 + 45	45 + 45
Powershift number of speeds	n°	3	3	3	3
Overspeed (40km/h Eco)		●	●	●	●
REAR PTO					
Clutch dry		-	-	-	-
Multiplate wet clutch		●	●	●	●
Mechanical engagement		-	-	-	-
electrohydraulic engagement		●	●	●	●
P.T.O. 540/540ECO		●	●	●	●
PTO 540/540ECO/1000		○	○	○	○
Ground speed PTO		○	○	○	○
FRONT PTO					
PTO 1000		○	○	○	○
FRONT AND REAR AXLE					
Front wheel drive mechanically engaged		-	-	-	-
Front wheel drive electrohydraulically engaged		●	●	●	●
Mechanically operated differential lock		-	-	-	-
Electrohydraulically operated differential lock		●	●	●	●
Steering angle 2WD	degrees	-	70°	70°	-
Steering angle 4WD	degrees	55°	55°	55°	55°
BRAKES					
4-wheel braking		●	●	●	●
Mechanical parking brake		●	●	●	●
Trailer hydraulic braking valve		○	○	○	○
STEERING					
Hydrostatic steering with independent pump		●	●	●	●
HITCH					
Rear hitch mechanical		●	●	●	●
Rear hitch electronic		○	○	○	○
Rear hitch lift capacity (std)	kg	3000	3000	3000	3000
Rear hitch lift capacity (opt)	kg	3600	3600	3600	3600
Front hitch		○	○	○	○
Front hitch capacity	kg	1750	1750	1750	1750
HYDRAULIC SYSTEM					
Pump output	l/min	54	54	54	54
Open centre hydraulic system		●	●	●	●
Rear auxiliary valves	n° of ways	6	6	6	6
CAB					
Air conditioning		○	○	○	○
High visibility roof		○	○	○	○
Suspension driver seat		●	●	●	●
DIMENSIONS AND WEIGHTS					
Standard rear tyres		420/70R30	420/70R30	480/70R30	480/70R30
Max. length without rear linkage	mm	3430	3430	3560	3560
Width (min./max.)	mm	1940/2240	1940/2240	2005/2405	2005/2405
Wheelbase (4WD/2WD)	mm	2100 / -	2100 / 2140	2230 / 2270	2230 / -
Front track (min./max.)	mm	1460/1760	1460/1760	1450/1850	1450/1850
Rear track (min./max.)	mm	1500/1800	1500/1800	1500/1900	1500/1900
Max. height at cab	mm	2460	2460	2485	2485
Weight with cab	kg	3200	3200	3300	3300
Maximum load capacity	kg	5200	5200	5200	5200

- STD
- OPT
- Not available

The above specifications refer to the highest specification package. For standard equipment and options refer to the current price list and ask your local dealer for details.

		80 Classic	90 Classic	90.4 Classic
ENGINE				
Emissions		Tier4i	Tier4i	Tier4i
Model		FARMotion	FARMotion	FARMotion
Cylinders/Displacement/Valves	No./cc/No.	3 / 2887 / 2	3 / 2887 / 2	4 / 3849 / 2
Common Rail	bar	2000	2000	2000
Turbo intercooler		●	●	●
Maximum power (ECE R 120) @ 2,000 rpm	kW/hp	55,4/75	65/88	65/88
Power at rated speed (ECE R 120) @2,200 rpm	kW/hp	55,4/75	61,6/84	61,6/84
Max. torque @ 1,600 rpm	Nm	341	354	354
Torque rise	%	40	32	32
Electronic engine control		●	●	●
Exhaust gas after-treatment system		DOC / exEGR	DOC / exEGR	DOC / exEGR
Fuel tank capacity	litres	75	75	75
SHUTTLE				
Mechanical Shuttle (LS)		●	●	●
Hydraulic reverse power shuttle (GS)		-	-	-
Stop&Go System (GS)		-	-	-
MECHANICAL GEARBOX				
Number of speeds	n°	10 + 10	10 + 10	10 + 10
Number of speeds with supercreeper gear and underdrive	n°	30 + 15	30 + 15	30 + 15
Overspeed (40km/h Eco)		-	-	-
POWERSHIFT GEARBOX				
Number of speeds with supercreeper gear	n°	-	-	-
Powershift number of speeds	n°	-	-	-
Overspeed (40km/h Eco)		-	-	-
REAR PTO				
Clutch dry		●	●	●
Multiplate wet clutch		-	-	-
Mechanical engagement		●	●	●
electrohydraulic engagement		-	-	-
P.T.O. 540/540ECO		●	●	●
PTO 540/540ECO/1000		○	○	○
Ground speed PTO		○	○	○
FRONT PTO				
PTO 1000		-	-	-
FRONT AND REAR AXLE				
Front wheel drive mechanically engaged		●	●	●
Front wheel drive electrohydraulically engaged		-	-	-
Mechanically operated differential lock		●	●	●
Electrohydraulically operated differential lock		-	-	-
Steering angle 2WD	degrees	-	70°	70°
Steering angle 4WD	degrees	55°	55°	55°
BRAKES				
4-wheel braking		●	●	●
Mechanical parking brake		●	●	●
Trailer hydraulic braking valve		○	○	○
STEERING				
Hydrostatic steering with independent pump		-	●	●
HITCH				
Rear hitch mechanical		●	●	●
Rear hitch electronic		-	-	-
Rear hitch lift capacity (std)	kg	2650	2650	2650
Rear hitch lift capacity (opt)	kg	3600	3600	3600
Front hitch		-	-	-
Front hitch capacity	kg	-	-	-
HYDRAULIC SYSTEM				
Pump output	l/min	42	42	42
Open centre hydraulic system		●	●	●
Rear auxiliary valves	n° of ways	4 / 6	4 / 6	4 / 6
CAB				
Air conditioning		○	○	○
High visibility roof		-	-	-
Suspension driver seat		●	●	●
DIMENSIONS AND WEIGHTS				
Standard rear tyres		420/70R30	420/70R30	480/70R30
Max. length without rear linkage	mm	3430	3430	3560
Width (min./max.)	mm	1940/2240	1940/2240	2005/2405
Wheelbase (4WD/2WD)	mm	2100 / -	2100 / 2140	2230 / 2270
Front track (min./max.)	mm	1460/1760	1460/1760	1450/1850
Rear track (min./max.)	mm	1500/1800	1500/1800	1500/1900
Max. height at cab	mm	2460	2460	2485
Weight with cab	kg	3200	3200	3300
Maximum load capacity	kg	5200	5200	5200

- STD
- OPT
- Not available

Area dealers



same-tractors.com

SAME is a brand of  SDF

